



THE GOODSYARD

Design Guide

September 2019 - Part 2 of 6



ballymore.



2.0 SITE-WIDE PARAMETERS

2.1 DESIGN PRINCIPLES

2.1.1 Masterplan Design Principles

The revised masterplan has evolved from a very clear set of design principles which have shaped the strategic approach to design. The key design principles are:

- Retain heritage to bring character and identity to the development;
- Create new routes to bring appropriate scale to development plots;
- Consideration of the specific and varied boundary conditions;
- Generosity of public realm over two levels;
- Create characterful, purposeful and usable spaces throughout;
- Consideration of the existing surrounding context with a series of independent urban blocks;
- To position massing and height where appropriate;
- To promote a rich mix of uses to create a vibrant new destination within the city.

These design principles have shaped a masterplan structure which has enabled the parameter set to be established. The following pages highlight the key components of the revised scheme to illustrate the principles which have been translated into the parameter drawings.



Fig 2.1.1: Artist sketch of the masterplan design intent

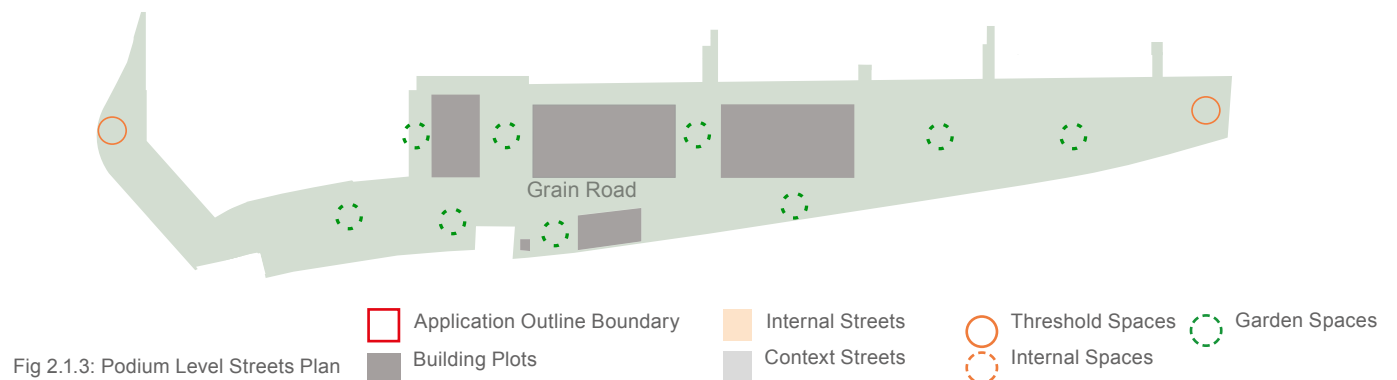
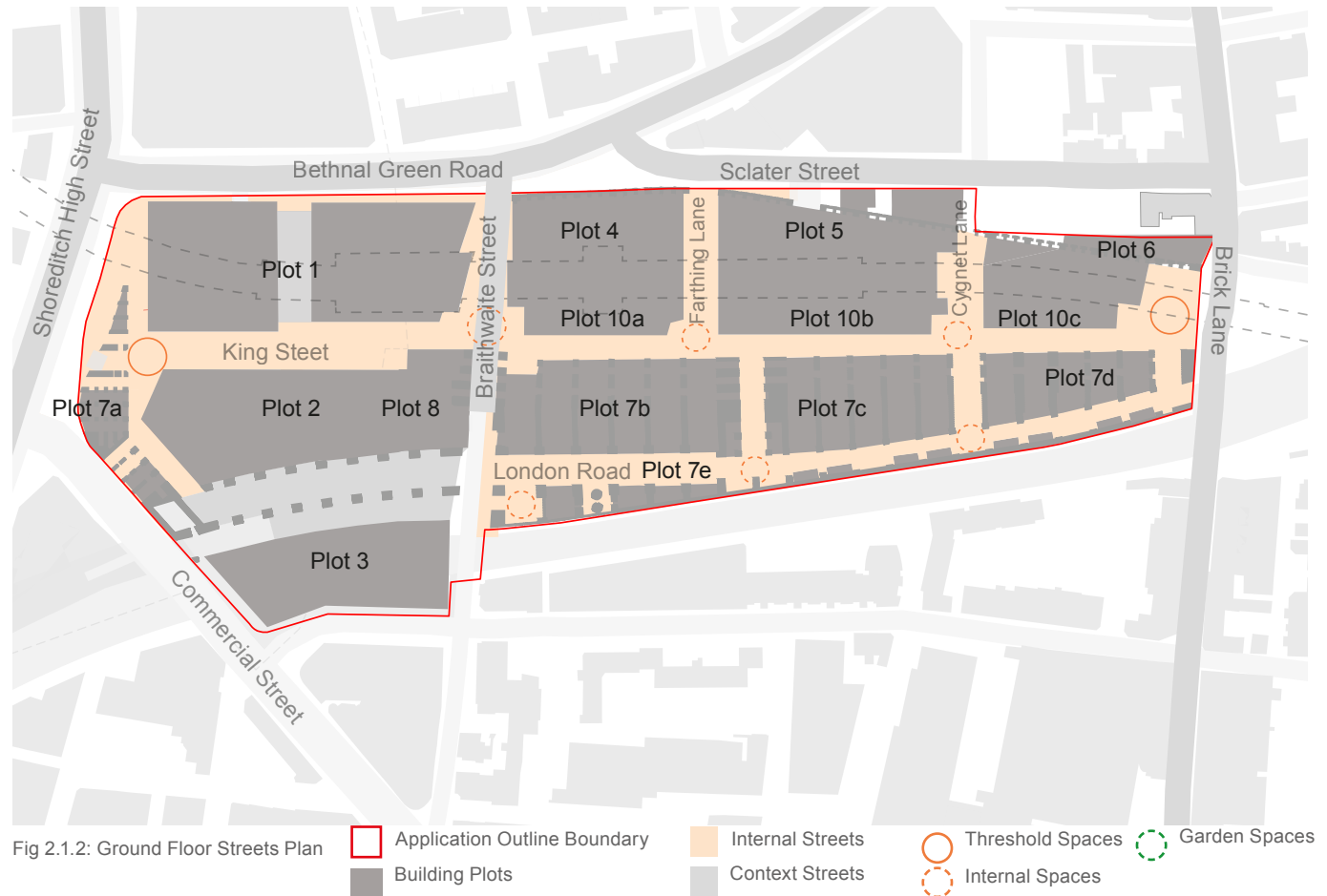
2.1.2 Masterplan Structure

The following diagrams illustrate the spatial typologies that have been created in the masterplan. The design of each space is covered in more detail in Section 2.2 – Character Area Guide. Below is an overview of these key spaces and the strategic approach applied to each.

Context streets – Specific consideration has been given to how the Goodsyard street environment weaves itself into the existing surrounding streetscape. The masterplan has considered how best it can improve the environment for people and how it can most effectively bring vibrancy and character to these spaces. The streets referred to as contextual are; Shoreditch High Street, Bethnal Green Road, Sclater Street, Brick Lane and Braithwaite Street.

Internal Streets – The masterplan has created a network of internal streets that connect the site into its context. These internal streets will each have their own characters and scale and have been named to record a significant historic influence. These streets are open to the sky where possible. The design intent is that these streets have an authentic and endearing character that overtime enables them to become an integrated part of this part of London's townscape. The primary east west route will be known as Kings Street with London Road retaining its current name, the north south routes will be known as Farthing Lane and Cygnet Lane.

Threshold spaces – Threshold spaces are key to ensuring that the integrity of the Bishopsgate Goodsyard is retained in a way that is legible in the context. To this end the 'reading' of the Goodsyard through its full east west extent is



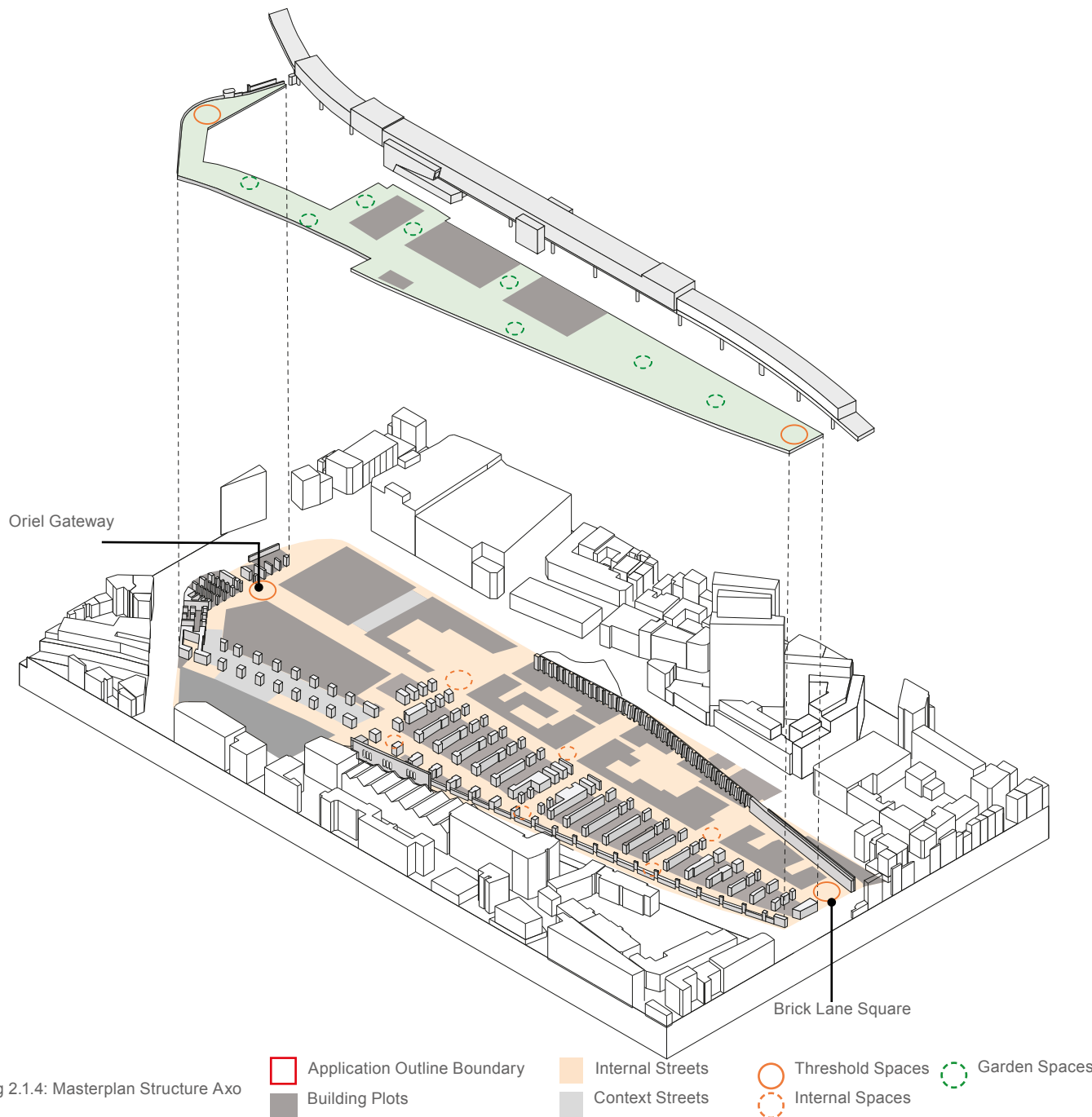


Fig 2.1.4: Masterplan Structure Axo

critical. To emphasise the significance of the existing structure we propose two primary threshold spaces one at Brick Lane and one around the Oriel Gateway. These spaces have been carefully scaled to ensure they are large enough to mark a moment in the city, whilst they are considered enough to not dominate the fine grain of the historic structures.

Internal Spaces – Internal spaces have been created where routes intersect or where there is a specific moment of interest that is worthy of celebration. Examples of these spaces are the interface between the east west route and the north south streets. Small internal squares are created by pushing façade frontage back into the arch structures. These spaces enable the public to engage positively with the grade II listed viaduct.

The proposed streets and lanes that dissect the masterplan improve permeability whilst breaking the scale of the proposed urban blocks down to a more contextually appropriate response.

Open Spaces - An important feature of the masterplan is the provision of additional open space to provide much needed recreational area for the residents of this part of London. Whilst additional open space has been accommodated at street level, the most significant recreational 'green' spaces are located at platform level.

Another important move has been the integration of building 2 with the Oriel Gateway and the Commercial Street wall. Opening up this threshold allowing people to access the site from the south delivers a new dimension and freedom to the way that people will be able to move around this part of the city.

The open space at platform level is to create a series of balconies, banks, gardens and fields providing up to 12,850m² of open space.

2.1.3 Masterplan Movement For People

The masterplan structure is designed to form a hierarchical network of streets, lanes, gateways and thresholds.

Vertical circulation to platform level is to be a theatrical experience, providing all users appropriate and inclusive connection to the upper level.

One of the key aims is to create a clear, legible circulation pattern and a high quality public realm. This approach to legibility will not only improve the legibility and clarity of the on-site circulation, but also create a vibrant and welcoming physical environment.

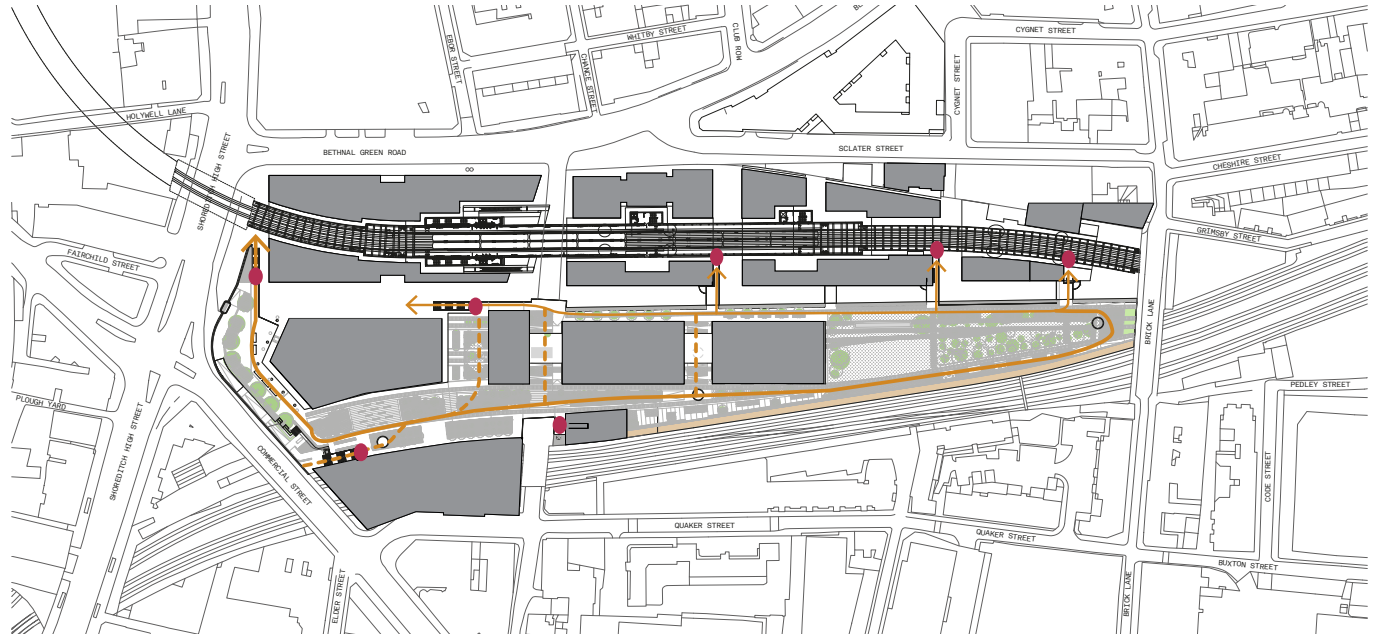


Fig 2.1.6: Platform plan showing movement for people

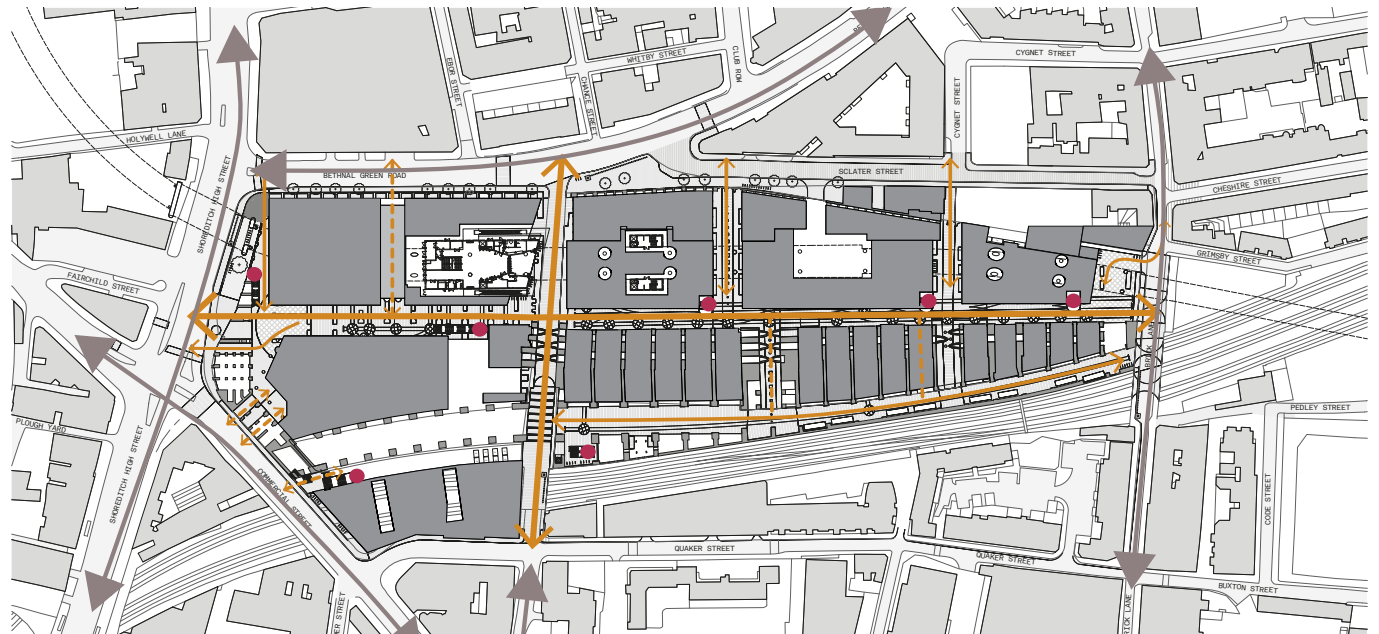


Fig 2.1.5: Ground floor plan showing movement for people

Building Plots Circulation Routes Vertical Circulation Points
Existing Primary Pedestrian Routes

2.1.4 Masterplan Engaged with Heritage

The masterplan is founded on the principle of retaining, reusing and making accessible existing heritage structures found on site. The masterplan aims to open up the most significant structure for the public to enjoy.

The northern edge of the Grade II listed Braithwaite Viaduct arches are to form and create the edge of the new east-west route through the centre of the scheme.

A number of existing historic buildings, on the northern boundary, are to be retained and re-furbished within Plot 5 (Weavers Cottages, Victorian Building and Mission hall). The spaces between will be given back to the street as a series of dwell spaces in generous public realm. The revised proposals retain the historic Boundary Wall and use it as a organizing devise behind which building mass can sit, but also repurpose it with active retail frontage alongside residential lobbies.

The Oriel Gateway is to be refurbished which will then serve as the main entrance to the site.

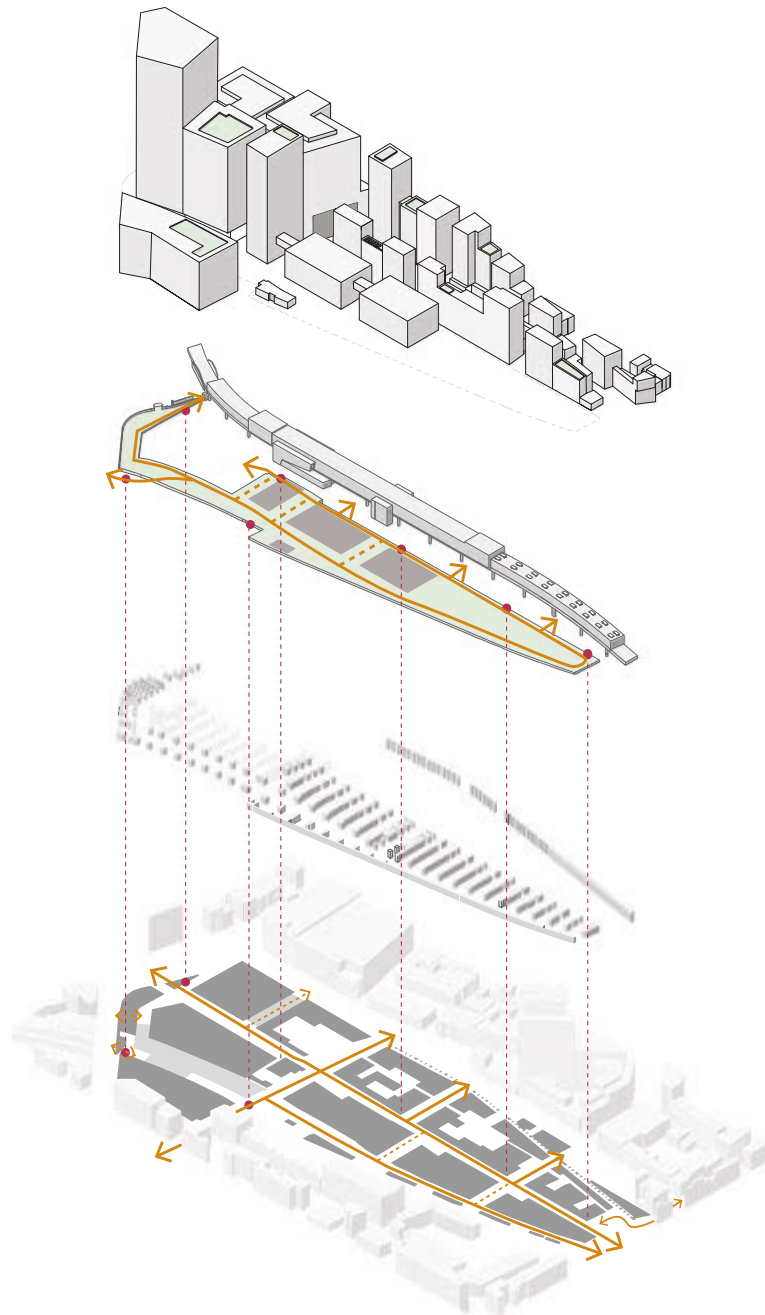


Fig 2.1.7: Exploded axonometric showing movement for people

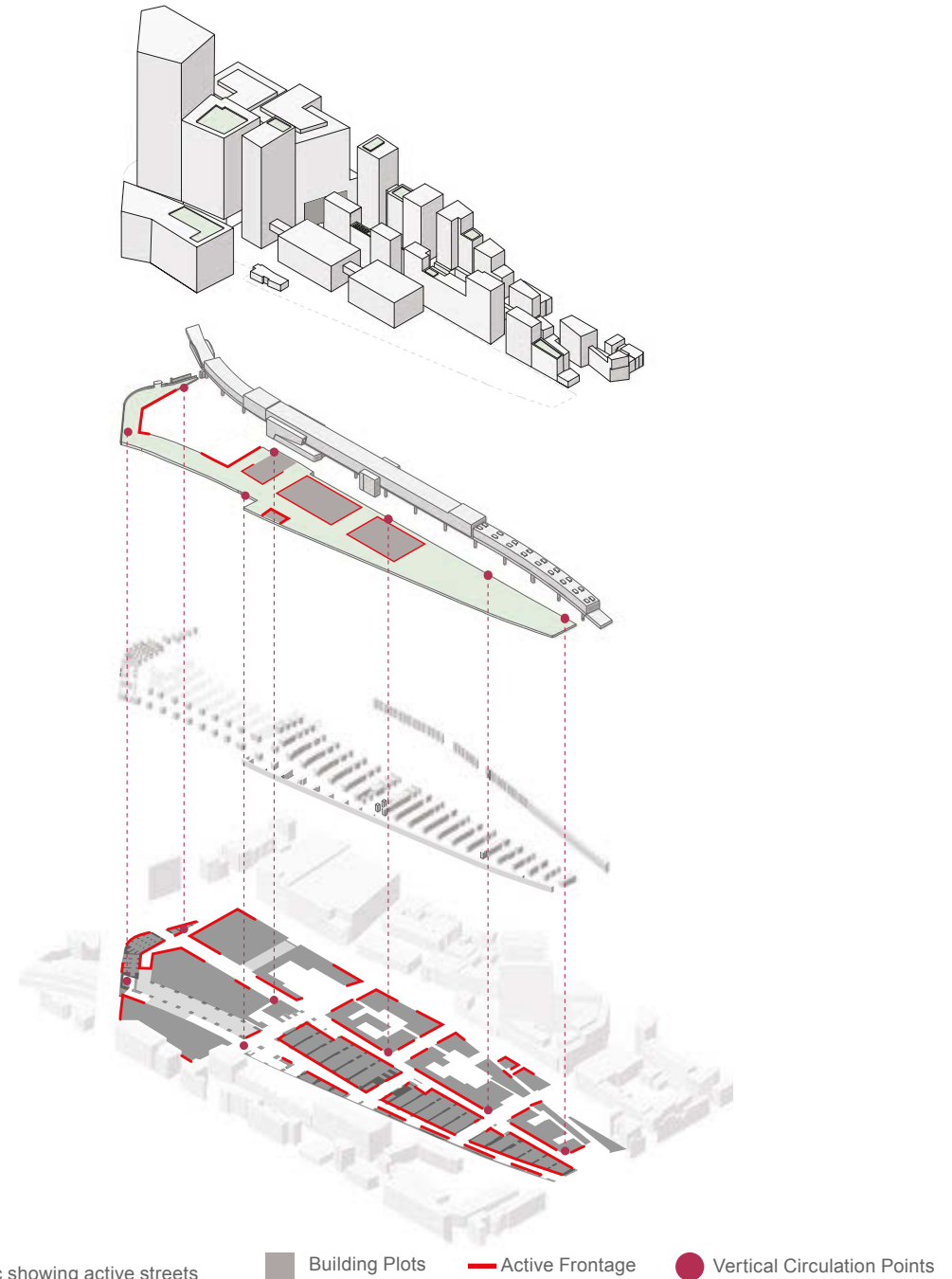
■ Building Plots ← Circulation Routes ● Vertical Circulation Points

2.1.5 Masterplan of Active Streets – Retail / Place Making Strategy

The site-wide retail approach has been shaped by two key aims:

- To animate the site's ground plane with engaging retail, food and beverage uses. At Platform level restaurants and eateries are proposed to make the best use of the generous, open, southerly aspect.
- To retain and imaginatively reuse the majority of the site's surviving historic structure.

The site develops its own unique, engaging and varied retail experiences, adopting a site-wide approach to animate connections, routes and public spaces. As such, each individual plot will provide their own retail elements at ground level. This will help to craft retail spaces of different characters and scales, encouraging a mix of tenants to take up residency in the Goodsyard.



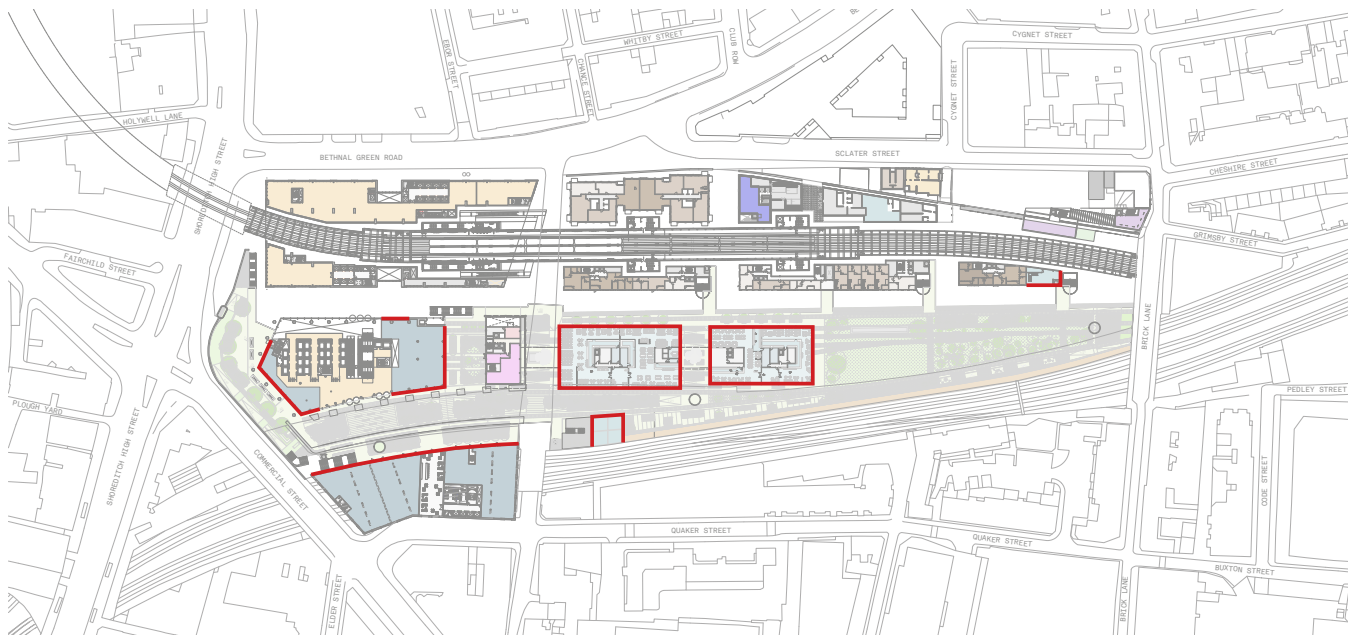


Fig 2.1.10: Platform plan showing active streets

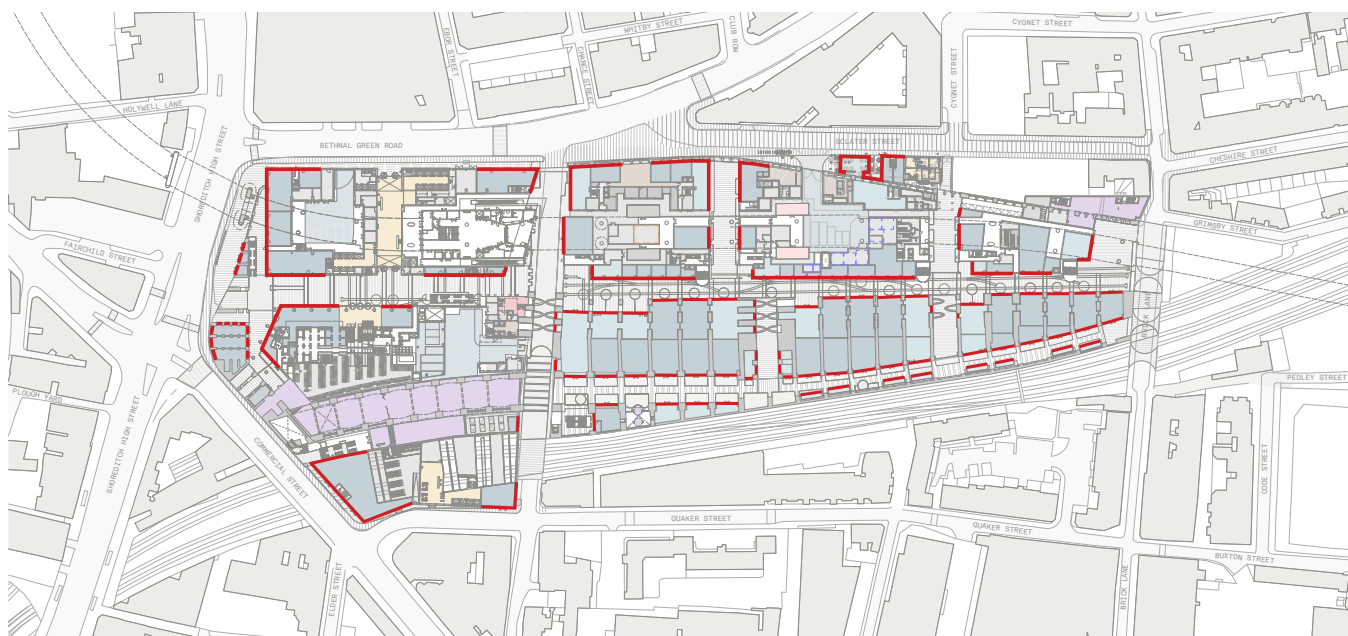


Fig 2.1.9: Ground floor plan showing active streets

2.2 CHARACTER AREA GUIDE

2.2.1 Introduction

The following section describes the key spaces and streets and focuses on the character for each. It sets out guidance describing approach to access, pedestrian movement, animation and urban integration. It is intended to protect the described masterplan principles in relation to the spaces between the defined plots, to protect the masterplan design intent as the individual plots come forward over time. It will also provide a further framework for the architecture of individual buildings, alongside the individual building guides found in section 4 of the document.

2.2.2 Oriel Gateway Character Area

The primary site entrance at the listed Oriel Gateway should be surrounded by active frontage.

The Oriel Gateway forms the primary western elevation to the Goodsyards. This will create an animated gateway arrival space and increase footfall.

The historic perimeter wall, the gates and the Oriel Bay feature should be surrounded by new public realm on all sides.

This elevation is characterised by the historic fabric. The separation from new build development will celebrate the existing heritage asset and allow pedestrian movement through and around it.

The Oriel feature should be celebrated as the main gateway into the new street and pedestrian flow should link into existing pedestrian routes on

Shoreditch High Street and Bethnal Green Road. This will be achieved by providing an increased generosity to public realm, increased footpath widths and new uses to building frontages lining the new routes.

This will ensure the new spaces will stitch into the existing urban environment by ensuring generous and inviting public spaces, encouraging public use and movement.

Webb Square, a new important threshold space at the west end of the new east west route, will be surrounded by active frontage as an important threshold space within the street and shall contain street furniture.

This will ensure the important space at the node of new routes is animated and offers opportunity for dwell time and orientation.

Access to the platform / Oriel balcony will be placed at arrival points within the masterplan and will be clear.

This will improve pedestrian connectivity to the platform level and help people orientate themselves within the masterplan by creating visual connections between ground and platform level.

The street width between plots 1 and 2 shall be as a minimum 13m in width and will be arranged to reference the historic layout of the Goodsyards.

This will ensure the street is an appropriate scale in relation to the proposed buildings and also offers a legible appreciation of the sites historic use.

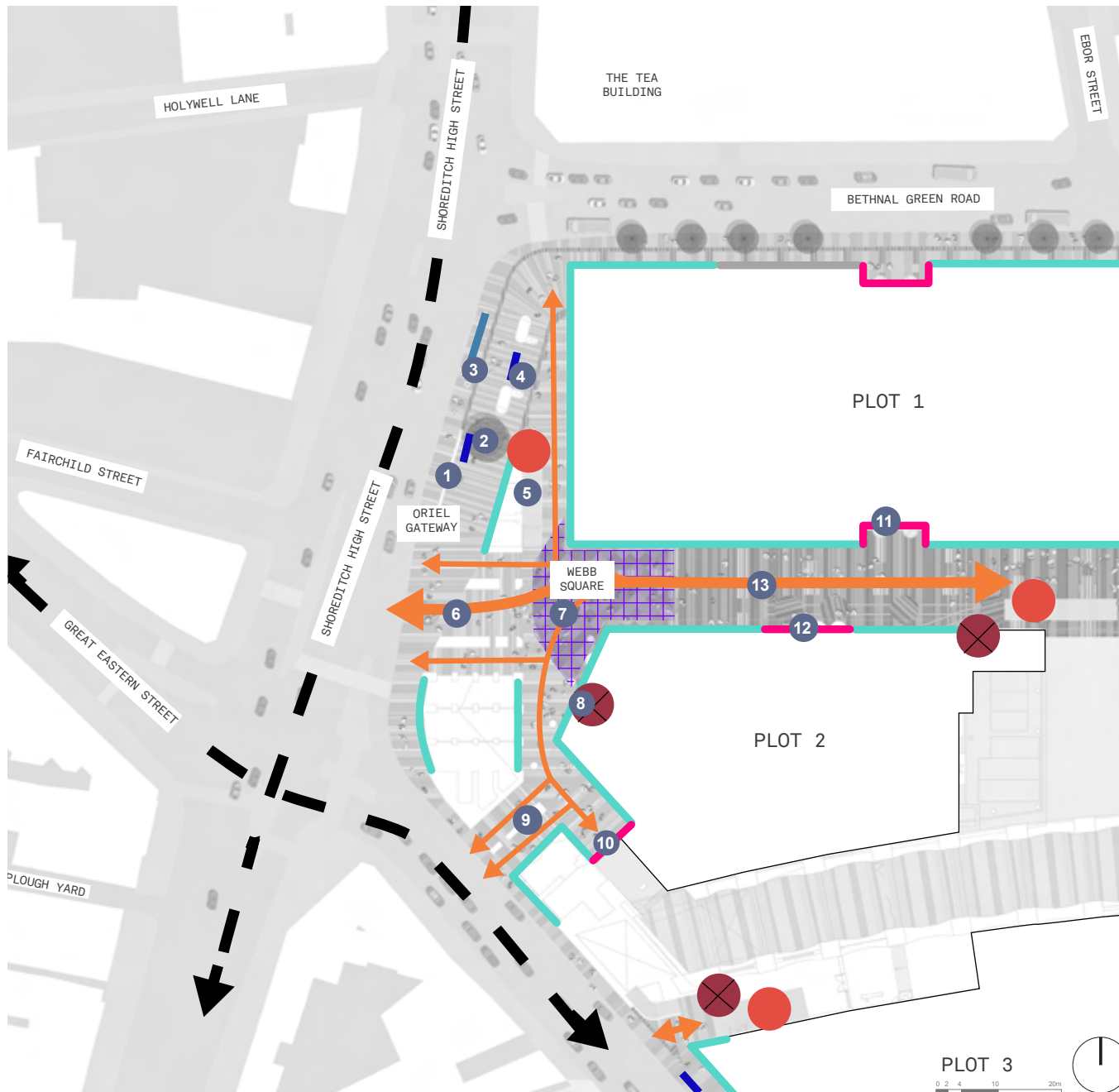


Fig 2.2.1: Plan: Oriol Gateway

KEY

1. Retained wall, gate pier and gates
2. Paving arrangement references the former inclined roadway to platform level and associated walls linking to retained wall and gates.
3. Proposed TfL docking station
4. Proposed visitor cycle stands
5. Oriel stairway to platform level
6. Main entrance formed of 3 open archways
7. Webb Square threshold space, paving references forecourt of the historic Shoreditch Station passenger terminus
8. Public lift within Building 2 to platform level
9. Commercial Street entrance formed of 2 open archways
10. Entrance to Events space and Building 2 cycle store
11. Building 1 entrance
12. Building 2 entrance
13. Middle Road, paving references Bishopsgate Goods Yard vault structure with truck distribution rails and turntables

- Key pedestrian movement through the city
- Key pedestrian movement through the site (Primary)
- Key pedestrian movement through the site (Secondary)
- Key threshold space
- Active frontage
- Building entrances
- Vertical circulation lifts
- Vertical circulation stairs
- Visitor cycle parking
- TfL cycle docking station
- Servicing Access

2.2.3 Bethnal Green Road Character Area

Active frontage will be provided to Bethnal Green Road and the corners of plot 1.

This is to encourage pedestrian flow where streets connect and provide access into the masterplan. Active frontage will help to animate the southern edge of Bethnal Green Road.

The public footpath on the south side of Bethnal Green Road should be widened from kerb edge to the building line of plot 1, as a single surface with tree planting.

This is to ensure a new environment along Bethnal Green Road is suitable for increased pedestrian flow and use of retail spaces along the pavement edge.

Building entrances should be clearly signposted and set in to allow further pavement width to the kerb edge.

This will allow space on the pavement for both building users at the entrance and pedestrian flow east west.

Shoreditch High Street Station should be clearly signposted and have a retained legible entrance from Braithwaite Street. The station entrance should face onto retail frontage.

It is important the sense of arrival from the station is considered and the station can be used for orientation within the masterplan.

- ► Key pedestrian movement through the city
- Shoreditch High Street Station
- Key pedestrian movement through the site (Secondary)
- Active frontage
- Building entrances
- TFL cycle docking station
- Vertical circulation lifts
- Vertical circulation stairs
- Visitor cycle parking
- TFL cycle docking station

KEY

- | | | | |
|----|--|----|--|
| 1. | Existing pedestrian crossing | 5. | New pedestrian crossing |
| 2. | Footway, paving references historic Goodsyard perimeter wall | 6. | Gateway entrance, framed by Building 2 and retained historic wall and viaduct structures |
| 3. | Service yard entrance | 7. | Shoreditch High Street Station entrance |
| 4. | Building 2 entrance & through route to Middle Road | | |

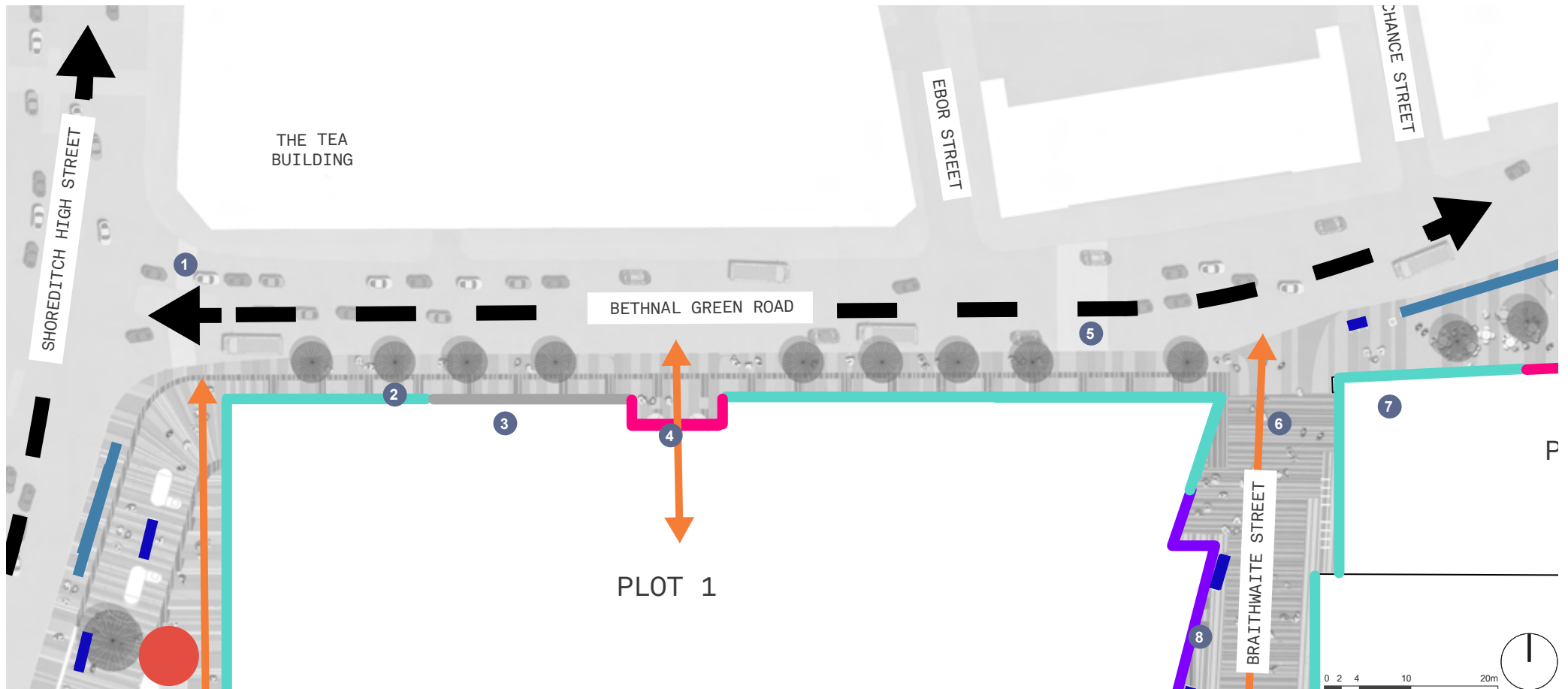


Fig 2.2.2: Plan: Bethnal Green Road

2.2.4 Sclater Street Character Area

Retail spaces will be provided along Sclater Street to provide active frontage.

This will animate the existing street edge and provide an outward facing northern edge to the masterplan.

Retail spaces should return into the north south streets within the masterplan, lining the street edge.

This will encourage pedestrian flow and permeability from existing surroundings into the new scheme and ensure streets have animation, activity and good surveillance.

New buildings will be set behind the existing Boundary Wall and not overhang or encroach forward of this. New public realm will infill the increased area between existing wall and kerb edge.












The buildings will respect the scale of the street and be subservient to the existing wall at street level. The increased public realm will allow for retail spill out along the northern edge.

Visitors cycle stands will be located in the increased areas of public realm.

This will locate cycles in areas of good visibility and surveillance with easy access to retail spaces and residential lobbies.

The Boundary Wall will be exposed along its length and will contain a diverse retail offer of shops, cafes and restaurants. Retail will also be included in the re-use of the Mission Hall and Victorian buildings.

The northern street edge is characterised by the existing structures. Refurbishing the spaces to create shop fronts will ensure the street edge has an outer face with the surrounding context.

-  Key pedestrian movement through the city
-  Shoreditch High Street Station
-  Key pedestrian movement through the site (Secondary)
-  Key threshold spaces
-  Active frontage
-  Building entrances
-  Vertical circulation lifts
-  Vertical circulation stairs
-  Cycle hub
-  Visitor cycle parking
-  TFL cycle docking station

KEY

- | | | | |
|----|--|-----|--|
| 1. | Existing TFL docking station retained | 6. | Service yard entrance |
| 2. | Existing tree retained | 7. | Cafe spill-out area to Mission Chapel |
| 3. | Taxi drop-off | 8. | Mission Hall |
| 4. | Threshold entrance through wall to Farthing Lane | 9. | Threshold entrance through wall to Cygnet Lane |
| 5. | Pavement of varying width | 10. | Residential entrances |

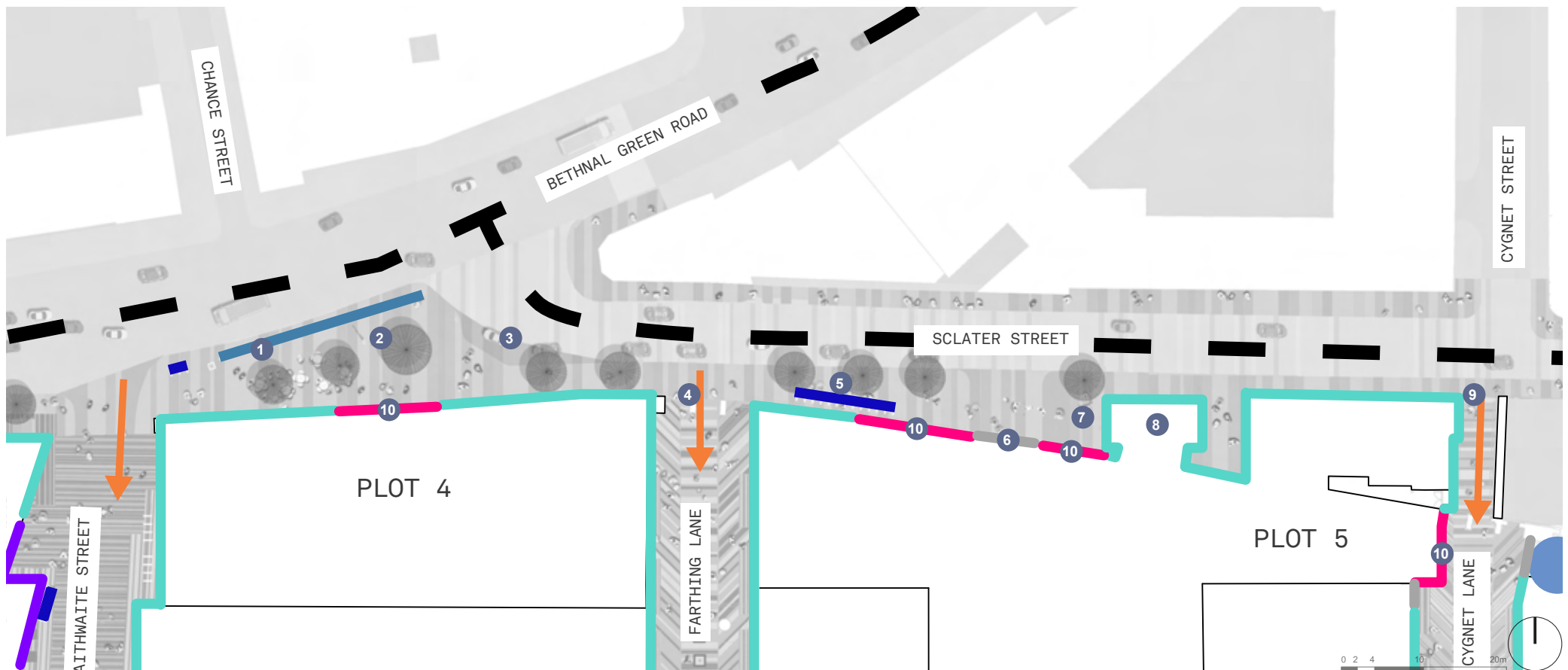


Fig 2.2.3: Plan: Sclater Street

2.2.5 Brick Lane Gateway Character Area

The existing non-listed boundary wall against Brick Lane to the north of the listed viaduct will be removed to open up a new threshold space on Brick Lane.

This will ensure the existing street environment is merged into the new development and increase public realm and pedestrian permeability at this end of the site.

The new buildings surrounding the square will be of moderate scale, relating to the existing Brick Lane context, and create enclosure around the new space. They will offer an active ground floor with opportunity for spill out and use of the public square.

The new square will mark an important threshold at the east end of the site and will be animated by surrounding public uses, as well as the existing uses on Brick Lane.

This new threshold space (King Square), will be surrounded by active frontage as an important threshold space within the street and shall contain street furniture.

This will ensure the important space at the node of new routes is animated and offers opportunity for dwell time and orientation.

The public vertical circulation point will be clearly visible and celebrated within the threshold space.

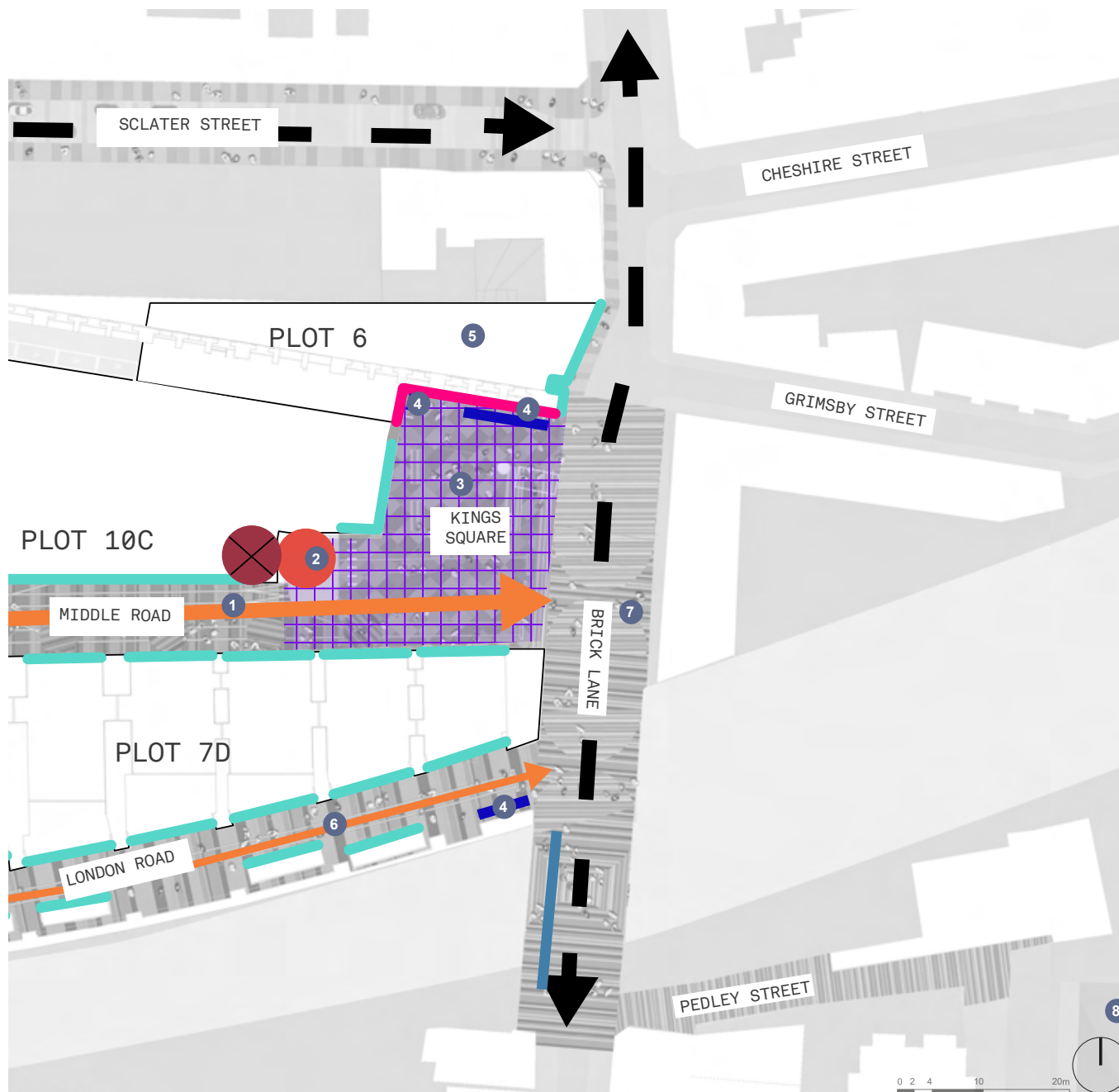
The connection between ground and platform level will be evident at all nodal points within the masterplan. Visibility from the public square is important for legibility and orientation.

The existing listed viaduct arches and London Road will be refurbished and re-used to provide active frontage along the new streets.

This will encourage pedestrian flow through the development from surrounding streets.

Landscaping on the platform level will be clearly visible from street level.

This will signpost the open public realm areas that exist at platform level.



KEY

1. Middle Road, paving reference to distribution rail and turntable of Goods Yard
2. Brick Lane Stair / Lift
3. King Square, paving reference to early 18th century square.
4. Visitor cycle stands
5. Cultural Building
6. London Road
7. Brick Lane, paving reference to demolished historic vault arches
8. Allen Gardens

- Key pedestrian movement through the city
- Key pedestrian movement through the site (Primary)
- Key pedestrian movement through the site (Secondary)
- Key threshold space
- Active frontage
- Building entrances
- Vertical circulation lifts
- Vertical circulation stairs
- Visitor cycle parking
- TFL cycle docking station

Fig 2.2.4: Plan: Brick Lane Gateway

2.2.6 Quaker Street / Commercial Street Character Area

Quaker Street shall have active frontage on the northern edge within plot 3.

This is an important outer face to the development and creating public frontage will help to animate the street .

The corner of Wheler Street and Quaker Street shall be marked by public use frontage aiding pedestrian flow into the new development.

Corner units are important to draw pedestrian flow around a corner and through Wheler Street up through Braithwaite Street.

The revised surfaces to Braithwaite/ Wheler Street should allow for a pedestrian, cycle and managed servicing environment.

High quality stone and clay paving will unify the public realm and will allow for all functions on Braithwaite/ Wheler Street, which acts as a primary north south route through the site.

The public realm should contain a new TfL docking station in a highly visible location.

This is aligned to the transport strategy. Cycle hire will be located on a primary route with good access and visibility.

The public vertical circulation point should be highly visible or marked by a feature to denote its location and celebrate the heritage.

Inclusive public access to platform level will be marked and legible on the street to ensure good connectivity between the two levels of public realm.

The street will contain appropriate lighting, signage, furniture and wayfinding.

This will ensure this primary route is legible and user friendly, 24 hours a day.

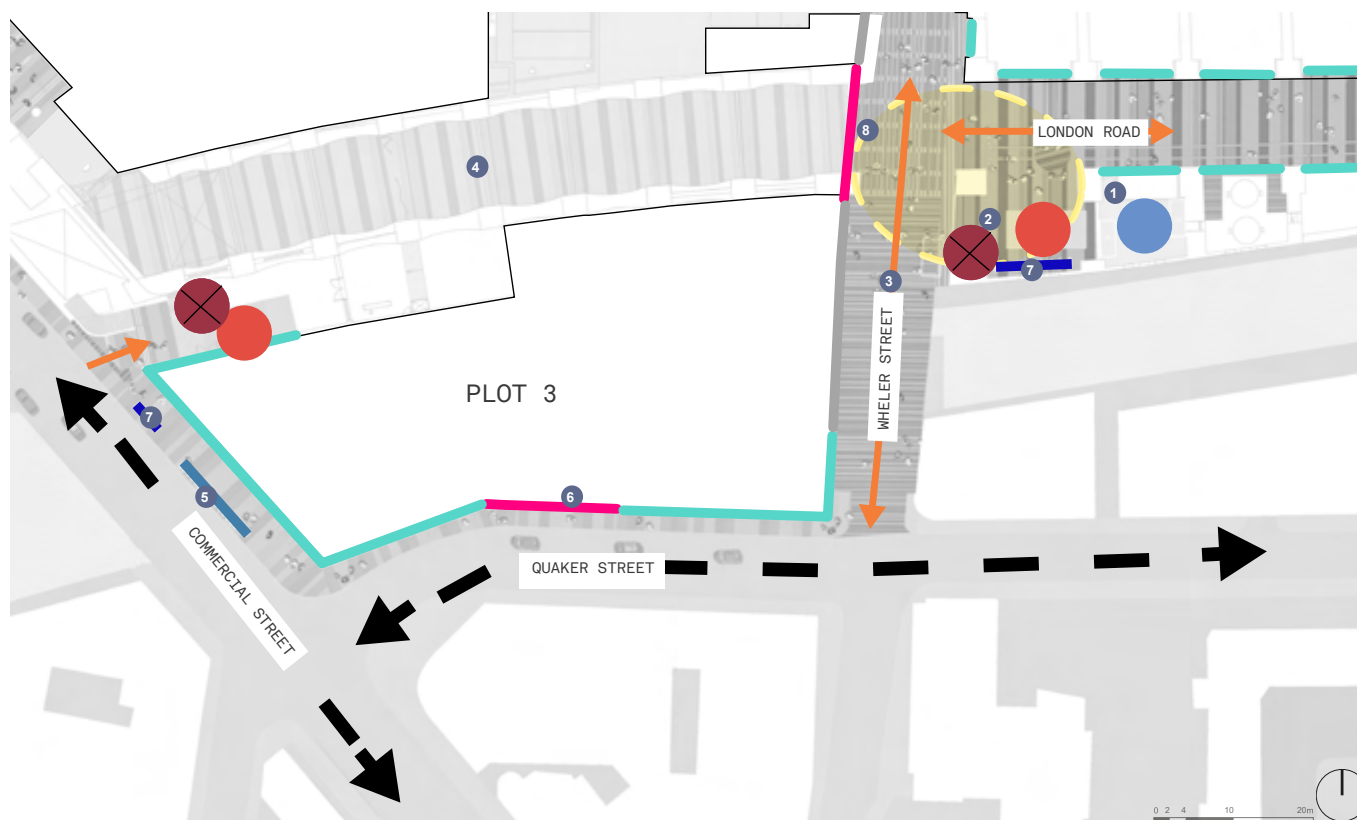


Fig 2.2.5: Plan: Quaker Street/Commercial Street

KEY

1. Cycle hub
2. Boiler Room Stairway & Lift within chimney
3. Braithwaite Street, paving references overhead jack arch structure of Goods Yard
4. Event space
5. TFL cycle docking station
6. Building 3 entrance
7. Visitor cycle parking
8. Events/Exhibition space entrance

- Key pedestrian movement through the city
- Key pedestrian movement through the site (Primary)
- Key pedestrian movement through the site (Secondary)
- Key nodal space
- Active frontage
- Building entrances
- Vertical circulation lifts
- Vertical circulation stairs
- Visitor cycle parking
- TFL cycle docking station
- Cycle hub

2.2.7 Middle Road

Middle Road is the primary east west link through the revised scheme and shall be an animated retail street with active frontage to both sides.

This will ensure the route is activated along its full length.

The public realm will respond to both the unique heritage features along the street and the spaces created between plots.

This will ensure there is an appropriate public realm strategy and a series of larger spaces at nodal junctions to break up the length of the street.

The street shall contain public seating and resting points.

This will help to define dwell areas and movement areas and provide public amenity within the street.

The public realm will allow for areas of breakout from retail spaces where the street width is increased.

Outdoor seating and resting spaces will help to characterise the street and provide variation in the animation and activity along its length.

The public vertical circulation points will be clearly visible and celebrated within the node spaces.

The connection between ground and platform level will be evident at all nodal points within the masterplan. Visibility along the public street is important for legibility and orientation.

The street width will be as a minimum 8m in width and will vary up to 16m along its length. The surface layout shall be arranged to reference the historic layout of The Goodsyard.

This will ensure the street is an appropriate scale in relation to the proposed buildings and also offers a legible appreciation of the sites historic use.

The planted edge of the platform level above the viaduct will be visible along the length of the street.

This will signpost the open public realm areas that exist at platform level.

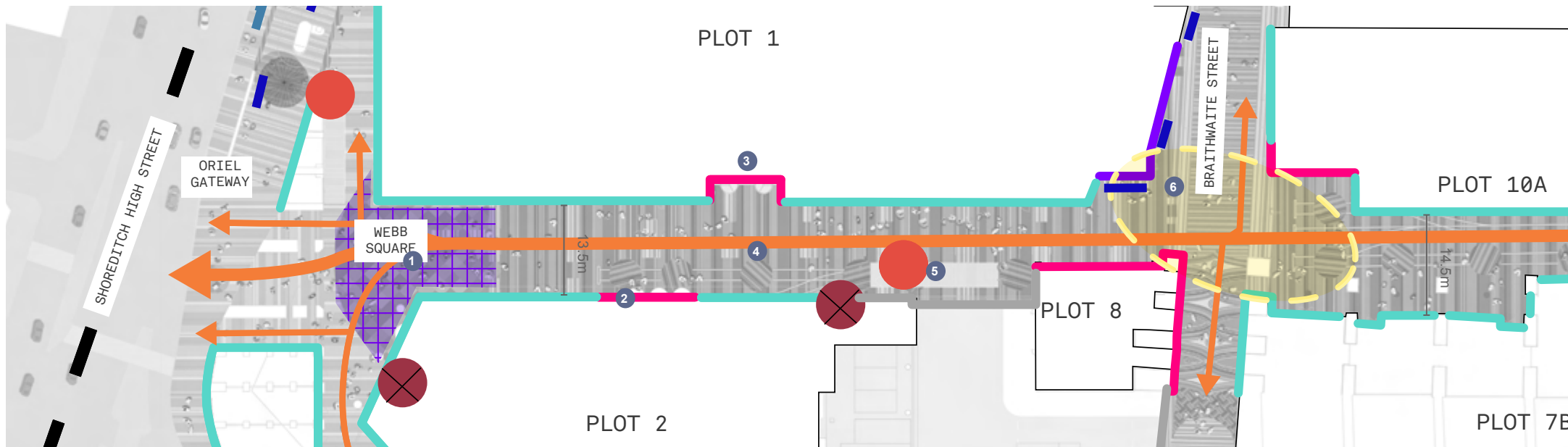
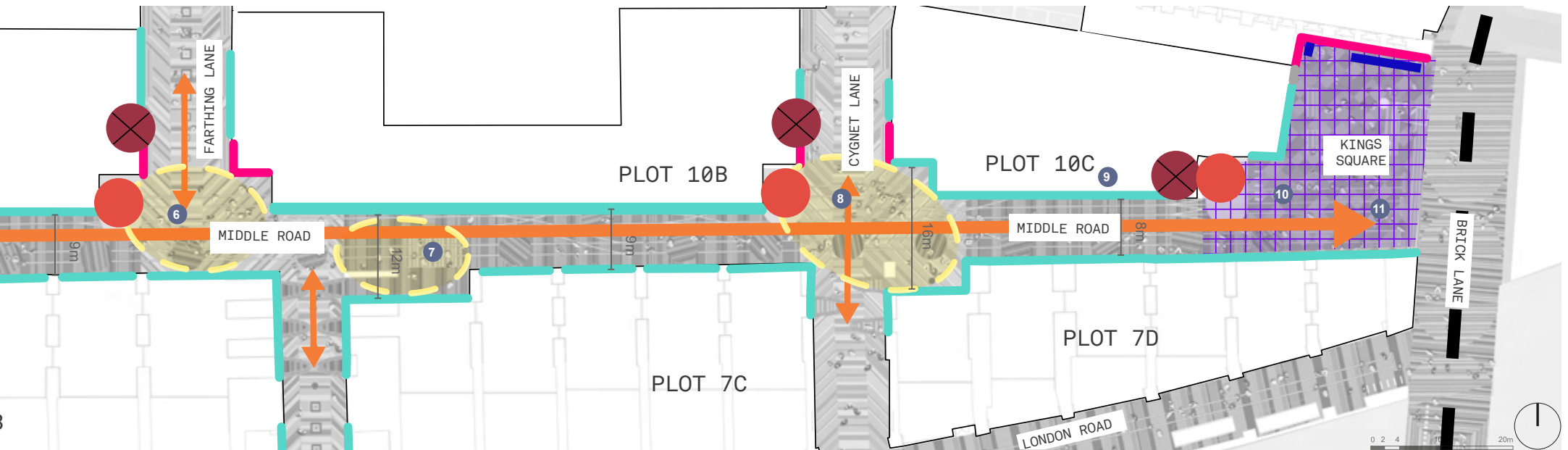


Fig 2.2.6: Plan: Middle Road

- ▶ Key pedestrian movement through the city
- ➡ Key pedestrian movement through the site (Primary)
- ➡ Key pedestrian movement through the site (Secondary)
- Key nodal space
- ▤ Key threshold spaces
- Active frontage
- Building entrances
- ⊗ Vertical circulation lifts
- Vertical circulation stairs
- Visitor cycle parking
- TFL cycle docking station
- Shoreditch High Street Station

KEY

1. Webb Square
2. Building 2 entrance
3. Building 3 entrance
4. Paving references Goods Yard distribution network of rails and turntables
5. Middle Road Stairway & Lift within Building 2
6. Farthing Lane Stairway & Lift
7. Cafe spill-out space, paving references Goods Yard truck hoist location
8. Cygnet Lane Stairway & Lift
9. Public toilets
10. Brick Lane Stairway & Lift
11. King Square



2.2.8 Braithwaite/Wheler Street Character Area

Braithwaite/Wheler Street is the primary north south connection through the development and shall provide for pedestrians, cycles and servicing vehicles south of the Braithwaite Viaduct.

The street is an existing route which will be upgraded to allow for the increased number of users.

The existing arches within the listed structure under the viaduct will be opened up and glazed to act as an entrance to the hotel and retail frontage.

This will help to improve the pedestrian environment under viaduct by providing active frontage and surveillance.

The public realm shall incorporate lighting, seating, cycle parking and access control.

This will be designed to incorporate management and security issues for the street and access into the development.

A new square shall be created at the junction of Braithwaite/ Wheler Street and Middle Road which will act as a primary area of public realm outside of the existing station. The space shall contain street furniture, cycle stands and will be animated by surrounding ground floor public use.

The increased public space will be generous at this important junction of the two primary pedestrian routes through the development, as well as providing space in front of the station entrance.

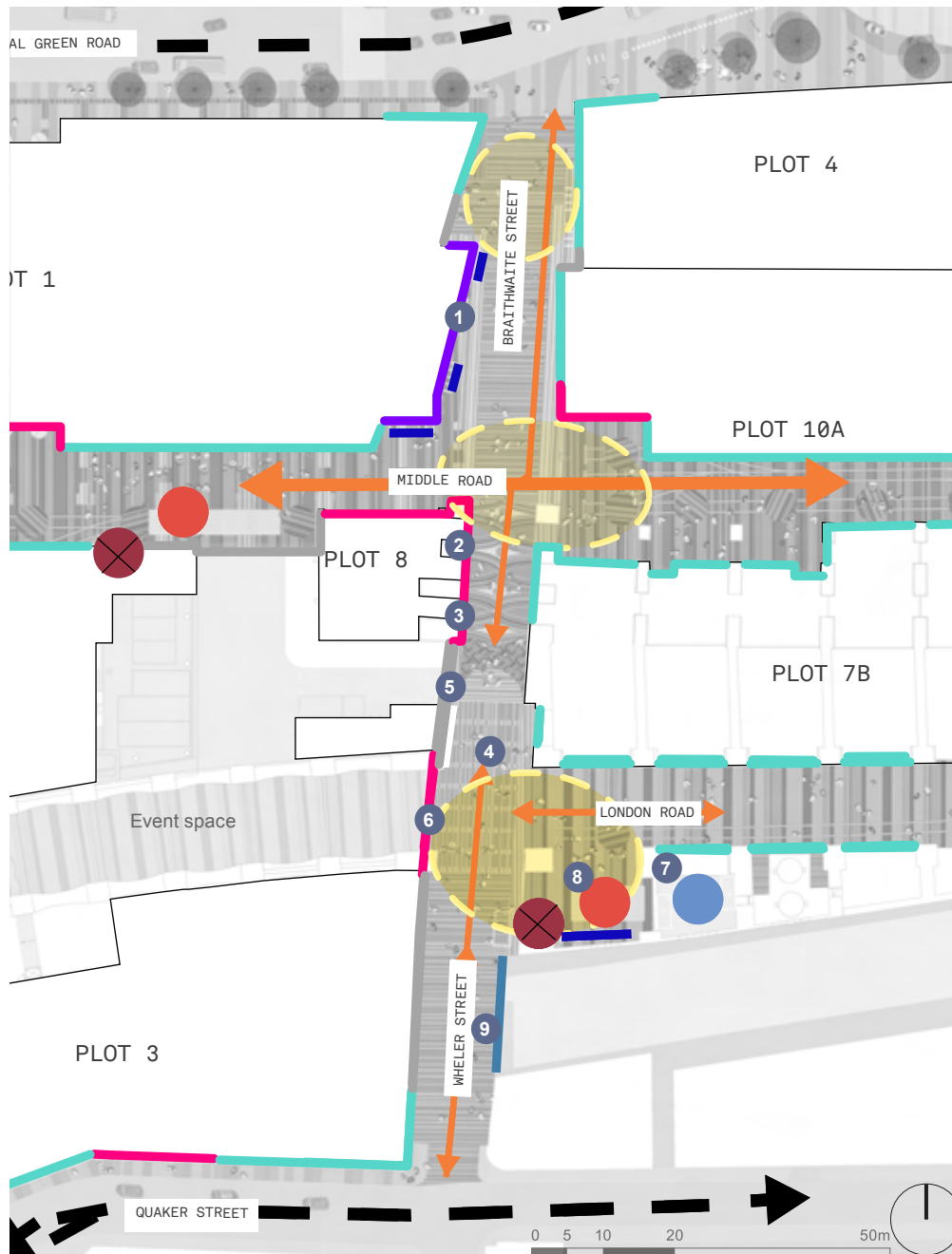


Fig 2.2.7: Plan: Braithwaite Street

KEY

1. Shoreditch High Street Station entrance
2. Building 8 Hotel lobby entrance
3. Building 8 Residential lobby entrance
4. Paving references historic structures including reflected cross arches, barrel vaults, jack arches and walls of the Goods Yard, providing contrast of colour texture and scale
5. Gates to entrance of service yard, part of family of gates around the site, woven metal design references history of Huguenot weavers
6. Entrance to Event Space
7. Cycle hub with public visitor cycle storage and associated amenity
8. Boiler Room Stairway & Lift
9. TFL cycle hire docking station

- Key pedestrian movement through the city
- Key pedestrian movement through the site (Primary)
- Key pedestrian movement through the site (Secondary)
- Key nodal space
- Shoreditch High Street Station
- Active frontage
- Building entrances
- Vertical circulation lifts
- Vertical circulation stairs
- Visitor cycle parking
- TFL cycle docking station
- Cycle hub

2.2.9 London Road / Farthing Lane / Cygnet Lane

The new north south lanes will be lined with active frontages.

This will create animated retail streets which will help to stitch the new development into the existing network of active streets.

Pedestrian routes connecting to London Road will have an open view through the existing structure at the route end.

Views of the city will improve legibility and way finding which will aid orientation.

Access into residential buildings will be placed along north south lanes.

Placing residential private frontages along secondary pedestrian network will keep primary routes active.

Access to platform level will be placed around transitional spaces.

This acts as a way finding strategy to enhance pedestrian linkages between ground and platform.

London Road will be brought into public use with upgraded surfaces and public frontages to arches lining the length. The south edge will retain the existing structure and dramatic southern light through the openings, but will also offer small retail kiosks lining the southern edge.

This is aligned to the vision statement to enhance and celebrate heritage. The kiosks have a dual function of providing a secure barrier to the railway cutting and creating variation in the user experience of the street.

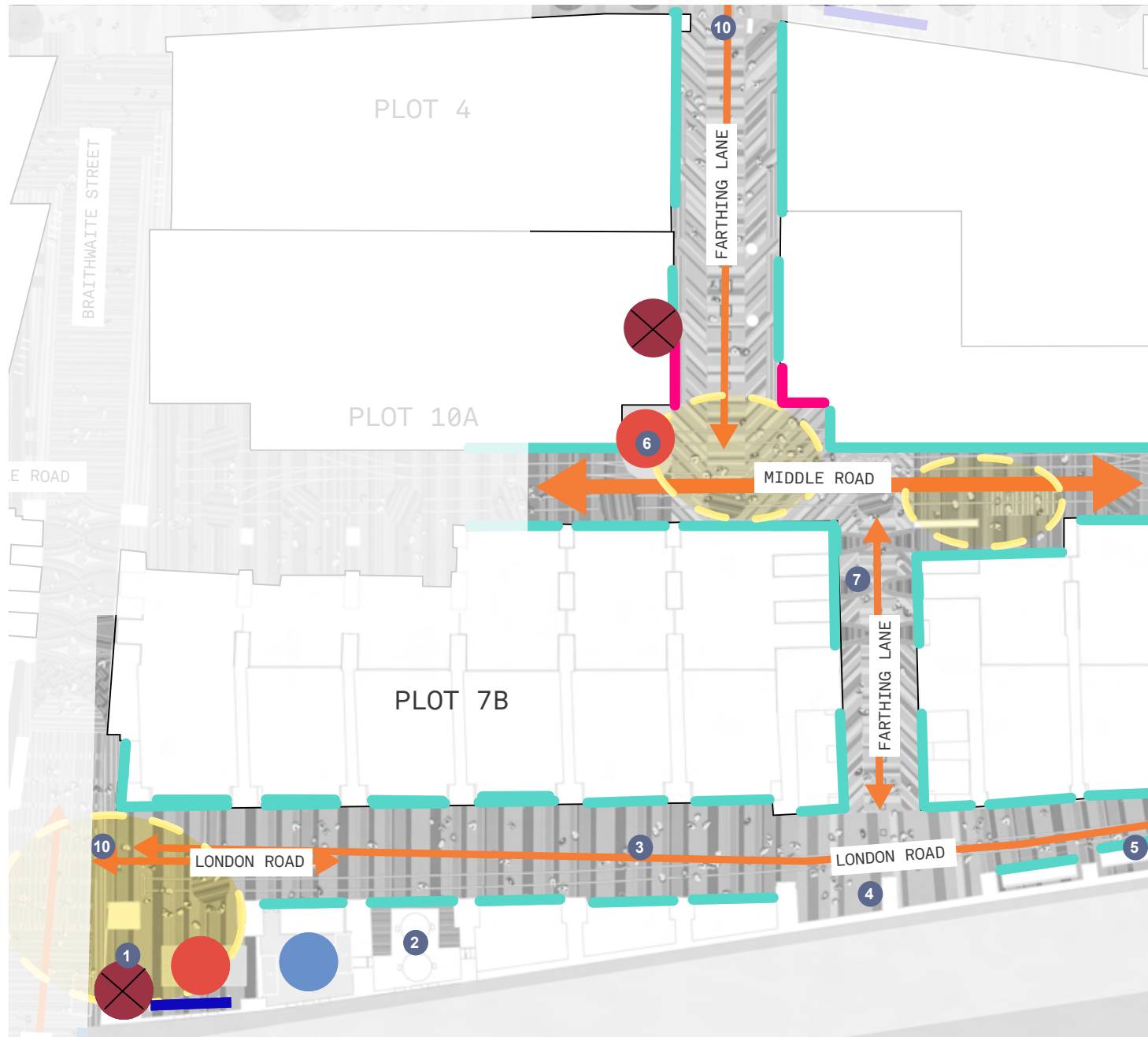
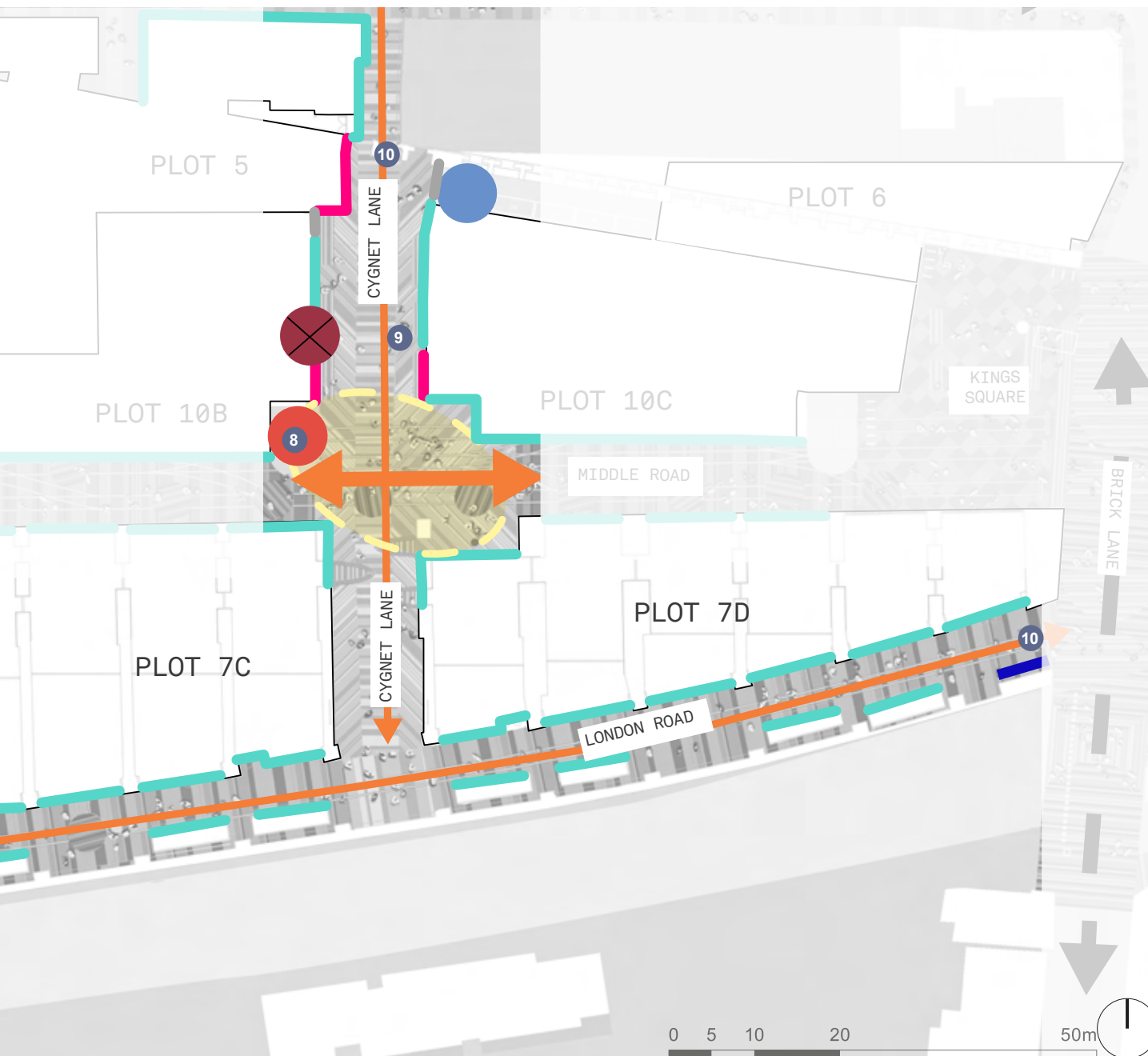


Fig 2.2.8: Plan: London Road



KEY

1. Boiler Room Stairway & Lift housed in chimney
2. Hydraulic accumulator viewing gallery and heritage interpretation centre
3. Existing granite paving restored, including historic rails, turntables and incidental rail paraphernalia
4. Open space to allow views out and light in
5. Retail pods
6. Farthing Lane Stairway & Lift
7. Farthing Lane paving woven arches, rails, turntables & oculi
8. Cygnet Lane Stairway & Lift
9. Cygnet Lane paving woven arches, rails, & turntables, canary yellow
10. Gates to control access if required

- Key pedestrian movement through the city
- Key pedestrian movement through the site (Primary)
- Key pedestrian movement through the site (Secondary)
- Key nodal space
- Active frontage
- Building entrances
- Vertical circulation lifts
- Vertical circulation stairs
- Visitor cycle parking
- Cycle hub